

ORDINANCE NO. 15- 99

AN ORDINANCE AMENDING THE ROGERS CITY CODE BY ADOPTING SECTION 14-46 OF THE ROGERS CITY CODE CODIFYING A COMPLETE STREETS POLICY; PROVIDING FOR THE EMERGENCY CLAUSE AND FOR OTHER PURPOSES.

WHEREAS, Pursuant to Arkansas Code Annotated §14-301-101 the City Council shall have the care, supervision, and control of all public highways, bridges, streets, alleys, public squares, and commons within the city.

WHEREAS, it is beneficial to the citizens of the City of Rogers that the City develop safe, reliable, efficient, integrated, and connected multimodal transportation system.

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF ROGERS, ARKANSAS:

Section 1: That Chapter 14 of the Code of Ordinances, City of Rogers, Arkansas, is hereby amended by adopting Section 14-46, as shown in Exhibit "A", attached hereto and incorporated by reference as if set out word for word herein.

Section 2: That the need to amend said City Code chapter is immediate and in order to protect the public peace, health, safety and welfare an emergency is hereby declared to exist and this Ordinance shall be in full force and effect from the date of its passage and approval.

Section 3: Severability Provision. In the event that any section, paragraph, subdivision, clause, phrase, or other provision or portion of this Ordinance shall be adjudged invalid or unconstitutional, the same shall not affect the validity of this Ordinance as a whole, or any part or provision, other than the part so decided to be invalid or unconstitutional, and the remaining provisions of this Ordinance shall be construed as if such invalid, unenforceable or unconstitutional provision or provisions had never been contained herein.

Section 4: Repeal of Conflicting Ordinances and Resolutions. All ordinances, resolutions or orders of the City Council, or parts of ordinances, resolutions or orders of the City Council in conflict herewith are hereby repealed to the extent of such conflict.

PASSED this 28th day of July, 2015.

APPROVED:

C. Greg Hines
C, GREG HINES,
Mayor

Attest:

Peggy David
PEGGY DAVID, City Clerk

Requested by: Lance Jobe

Prepared by: Chris Griffin, Senior Staff Attorney



EXHIBIT "A"

Sec. 14-46. Complete Streets

(a) Definition

"Complete Streets" means streets that are designed and operated to enable safe access for all users, in that pedestrians, bicyclists, motorists and public transportation users of all ages and abilities are able to safely move along and across a street.

(b) Policy

The City shall develop a safe, reliable, efficient, integrated and connected multimodal transportation system that will promote access, mobility and health for all users, and will promote that the safety and convenience of all users of the Public transit, people of all ages and abilities, motorists, emergency responders, freight providers and adjacent land users.

(c) Scope of Applicability

1. All City-owned transportation facilities in the public right-of-way including, but not limited to, streets, bridges and all other connecting pathways shall be designed, constructed, operated, and maintained so that users of all ages and abilities can travel efficiently and in an independent manner.
2. Privately constructed streets and parking lots shall adhere to this policy.
3. The City shall foster partnerships with the State of Arkansas, Benton County, Northwest Arkansas Regional Planning Commission, Northwest Arkansas Council, neighboring communities, business and school districts to develop facilities and accommodations that further the City's complete streets policy and continue such infrastructure beyond the City's borders as allowed by law.
4. The City shall approach every transportation improvement and project phase as an opportunity to create safer, more accessible streets for all users. These phases include, but are not limited to: planning, programming, design, right-of-way acquisition, construction, construction engineering, reconstruction, operation and maintenance. Other changes to transportation facilities on streets and rights-of-way, including capital improvements, re-channelization projects and major maintenance, must also be included.

(d). Exceptions

1. Any exception to this policy, including for private projects, must be reviewed and approved by both the Director of Planning and Transportation and the Senior Planner and be documented with supporting data that indicates the basis for the decision. Such documentation shall be publicly available.
2. A request for an exception that is denied under Section a. above may be appealed to the Planning Commission by filing with the City Clerk within 10 days' time.
3. Exceptions may be considered for approval when:

- i. An affected roadway prohibits, by law, use by specified users (such as an interstate freeways or pedestrian malls), in which case reasonable attempts shall be made to accommodate those specified users elsewhere, including on roadways that cross or otherwise intersect with the affected roadway;
- ii. The activities are ordinary maintenance activities designed to keep assets in serviceable condition (e.g. mowing, cleaning, sweeping, spot repair and surface treatments such as chip seal or interim measures);
- iii. The Director of Planning and Transportation or their designee issues a documented exception concluding that the application of Complete Streets principles is unnecessary, unduly cost prohibitive, or inappropriate because it would be contrary to public safety; or
- iv. Other available means or factors indicate an absence of need, including future need.

(e). Design Standards

The City shall follow nationally recognized design standards and use the best and latest design standards available. These standards include, but are not limited to: ITE Designing Walkable Urban Thoroughfares: A Context Sensitive Approach; AASHTO Guide for Development of Bicycle Facilities, AASHTO Guide for Planning, Designing and Operating Pedestrian Facilities, NACTO Urban Street Design Guide, and the NACTO Urban Bikeway Design Guide.

In recognition of context sensitivity, public input and the needs of many users, a flexible, innovative and balanced approach that follows other appropriate design standards may be considered, provided that a comparable level of safety for all users is present.

(f). Performance Measures and Reporting

1. The City shall measure the success of this Complete Streets policy using, but not limited to, the following performance measures:
 - i. Total miles of bike lanes
 - ii. Linear feet of new pedestrian accommodation
 - iii. Number of new curb ramps installed along city streets
 - iv. Crosswalk and intersection improvements
2. An annual report will be made to the Rogers Mayor and City Council by the Director of Planning and Transportation or their designee showing progress made in implementing this policy. The annual report on the annual increase or decrease for each performance measure contained in this ordinance compared to the previous year(s) shall be posted on-line for each of the above measures.

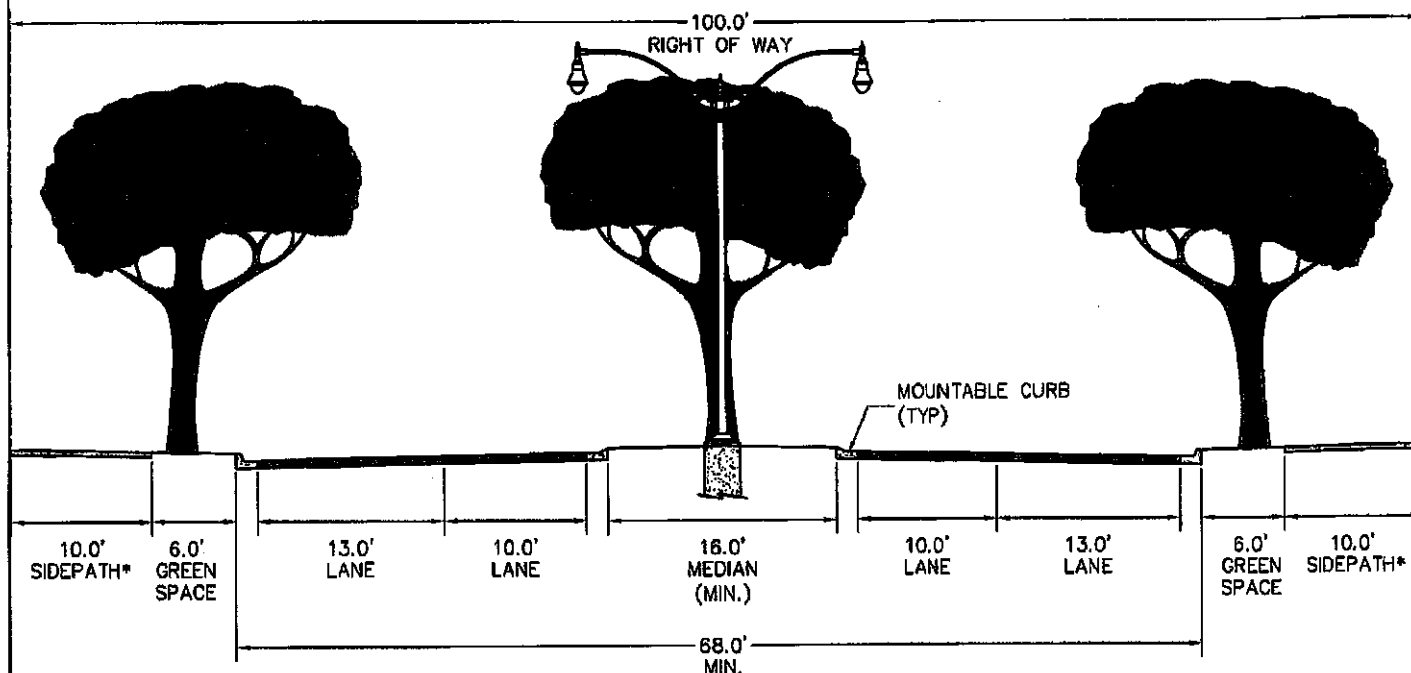
(g). Implementation

1. The City of Rogers shall view Complete Streets as integral to everyday transportation decision-making practices and processes. To this end:
 - i. The Department of Planning and Transportation, and other relevant departments, agencies, or committees will incorporate Complete Streets principles into all existing plans, manuals, checklists, rules, regulations, and programs as appropriate (including, but not limited to the Downtown Revitalization effort, the Comprehensive Growth Plan, Bond election programming, and other appropriate plans);
 - ii. The Department of Planning and Transportation, and other relevant departments, agencies, or committees will review current design standards, including subdivision regulations which apply to new roadway construction, to ensure that they reflect the best available design standards and guidelines, and will implement Complete Streets, where feasible;
 - iii. When available, the City shall encourage staff professional development and training on non-motorized transportation issues;
 - iv. City staff shall identify all current and potential future sources of funding for street improvements and recommend improvements to the project selection criteria to support Complete Streets projects;
 - v. The City shall promote inter-departmental project coordination among City departments with an interest in the activities that occur within the public right-of-way in order to better use fiscal resources;
 - vi. Complete Streets project shall include signage and striping to ensure that all users of the transportation system understand and can safely utilize Complete Streets project elements as appropriate.

(h). Typical Sections – See 1-6 for Detail

Exhibit A

MAJOR ARTERIAL STREET



* SIDEPATHS SHALL BE CONCRETE AND MATCH THE TRAIL DETAIL FOR JOINTING AND BASE EXCEPT EXPANSION JOINTS SHALL BE SPACED EVERY 30'.

1. STANDARD HEIGHT TO THE STREET LIGHT HEAD SHALL BE 30 FEET (TYP).
2. STREET LIGHTS SHALL BE PELCO MAJESTIC SERIES ROADWAY POLES WITH HARRISBURG SERIES LED LUMINAIRES, OR STAFF APPROVED EQUAL, SPACED A MAXIMUM OF 200 FEET APART AND AT INTERSECTIONS. POLES AND LUMINAIRES SHALL BE POWDER COATED BRONZE FEDERAL STANDARD COLOR 20062.
3. PAVING SECTION DETERMINED BY GEOTECHNICAL ENGINEER. BASE COURSE TO EXTEND 1' BEYOND CURB.
4. ALL CURB IS 1.5' WIDE.
5. THE OUTSIDE LANE EACH DIRECTION SHALL BE STRIPED WITH A SHARROW CENTERED 3' OFF THE FACE OF CURB AND SPACED EVERY 300'.
6. RAMP DRIVEWAYS UP TO MEET SIDEWALKS AND SIDEPATHS.

40-45 MPH
100' ROW



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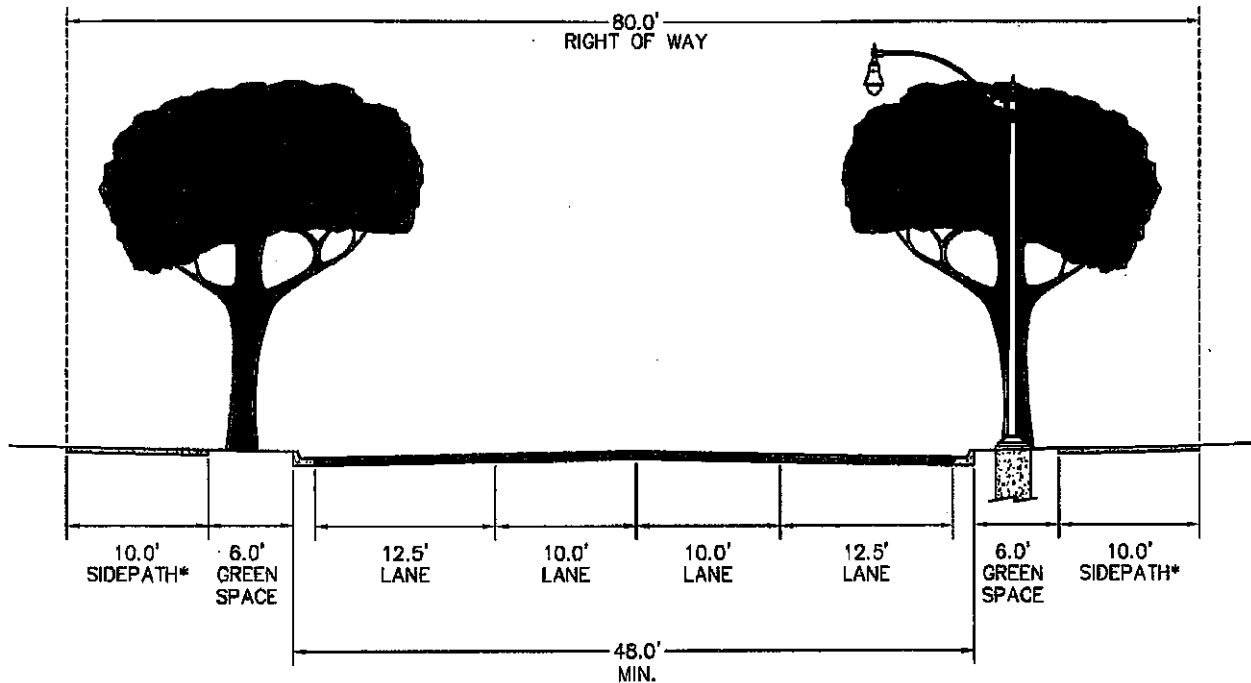
TYPICAL STREET SECTIONS

1 OF 6

NOT TO SCALE

JULY 14, 2015

MINOR ARTERIAL STREET



* SIDEPATHS SHALL BE CONCRETE AND MATCH THE TRAIL DETAIL FOR JOINTING AND BASE EXCEPT EXPANSION JOINTS SHALL BE SPACED EVERY 30'.

1. STANDARD HEIGHT TO THE STREET LIGHT HEAD SHALL BE 30 FEET (TYP).
2. STREET LIGHTS SHALL BE PELCO MAJESTIC SERIES ROADWAY POLES WITH HARRISBURG SERIES LED LUMINAIRES, OR STAFF APPROVED EQUAL, SPACED A MAXIMUM OF 200 FEET APART AND AT INTERSECTIONS. POLES AND LUMINAIRES SHALL BE POWDER COATED BRONZE FEDERAL STANDARD COLOR 20062.
3. PAVING SECTION DETERMINED BY GEOTECHNICAL ENGINEER. BASE COURSE TO EXTEND 1' BEYOND CURB.
4. ALL CURB IS 1.5' WIDE.
5. THE OUTSIDE LANE EACH DIRECTION SHALL BE STRIPED WITH A SHARROW CENTERED 3' OFF THE FACE OF CURB AND SPACED EVERY 300'.
6. RAMP DRIVEWAYS UP TO MEET SIDEWALKS AND SIDEPATHS.

35-40 MPH
80' ROW



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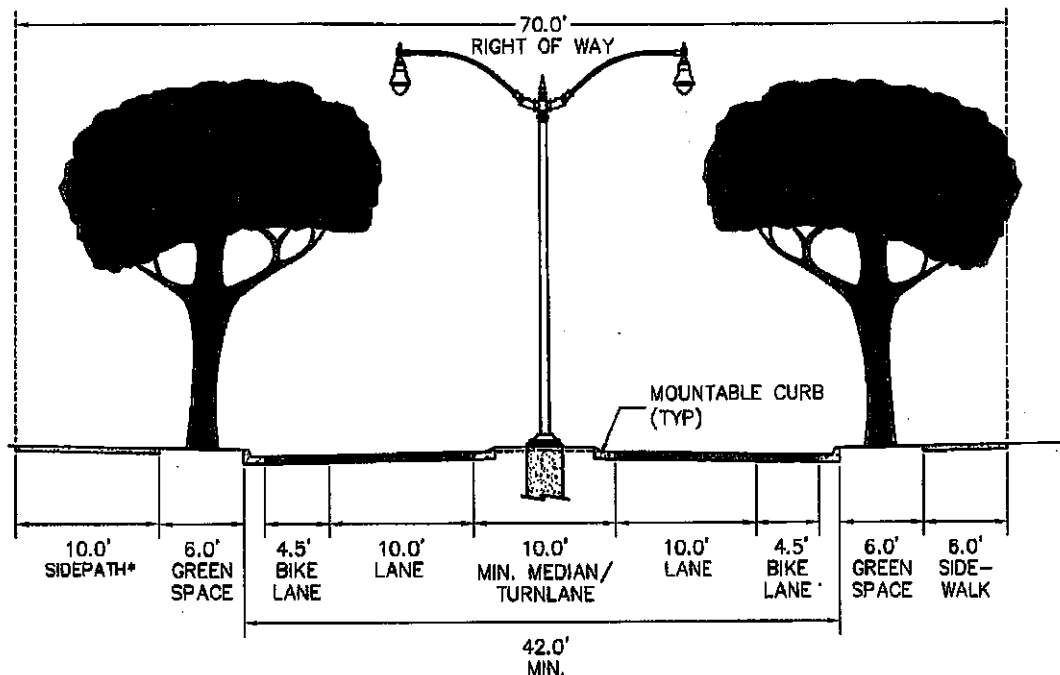
TYPICAL STREET SECTIONS

2 OF 6

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JULY 14, 2015

COLLECTOR STREET



*MAY BE ON EITHER SIDE OF THE ROAD BUT SHALL MATCH EXISTING SIDEPATH IF THERE IS ONE ALONG THE CORRIDOR ALREADY. COORDINATE LOCATION WITH PLANNING AND TRANSPORTATION DEPARTMENT. SIDEPATHS SHALL BE CONCRETE AND MATCH THE TRAIL DETAIL FOR JOINTING AND BASE EXCEPT EXPANSION JOINTS SHALL BE SPACED EVERY 30'.

1. STANDARD HEIGHT TO THE STREET LIGHT HEAD SHALL BE 30 FEET (TYP).
2. STREET LIGHTS SHALL BE PELCO MAJESTIC SERIES ROADWAY POLES WITH HARRISBURG SERIES LED LUMINAIRES, OR STAFF APPROVED EQUAL, SPACED A MAXIMUM OF 200 FEET APART AND AT INTERSECTIONS. POLES AND LUMINAIRES SHALL BE POWDER COATED BRONZE FEDERAL STANDARD COLOR 20062.
3. PAVING SECTION DETERMINED BY GEOTECHNICAL ENGINEER. BASE COURSE TO EXTEND 1' BEYOND CURB.
4. ALL CURB IS 1.5' WIDE.
5. IF A THREE LANE WITHOUT MEDIAN IS APPROVED, THE STREET LIGHTS SHALL BE ON ONE SIDE OF THE ROAD ONLY AND CENTERED IN THE GREENSPACE.
6. BIKE LANE STRIPING SHALL CONSIST OF A SOLID WHITE LINE TO DELINEATE THE LANE AND THE BIKE SYMBOL SHALL BE PLACED EVERY 300'.
7. RAMP DRIVEWAYS UP TO MEET SIDEWALKS AND SIDEPATHS.

30-35 MPH
70' ROW



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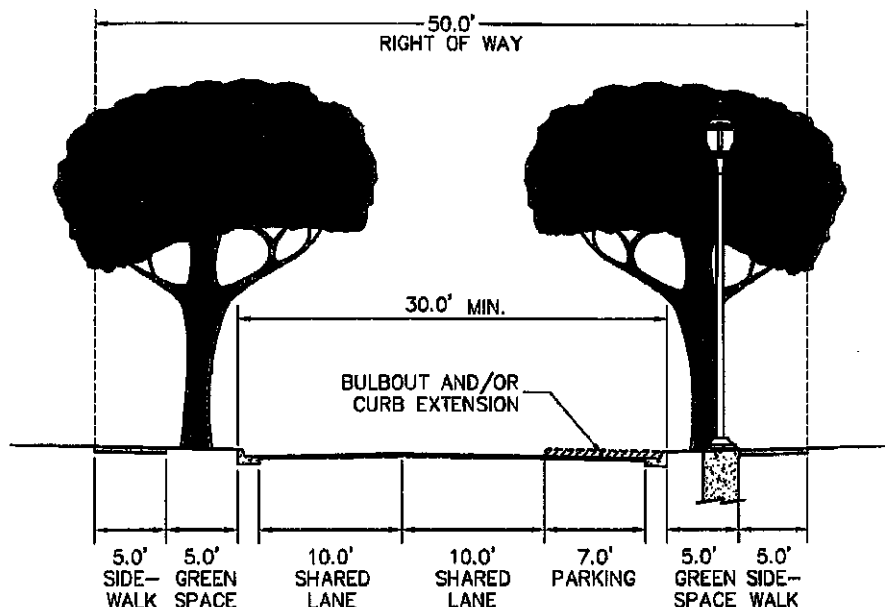
TYPICAL STREET SECTIONS

3 OF 6

NOT TO SCALE

JULY 14, 2015

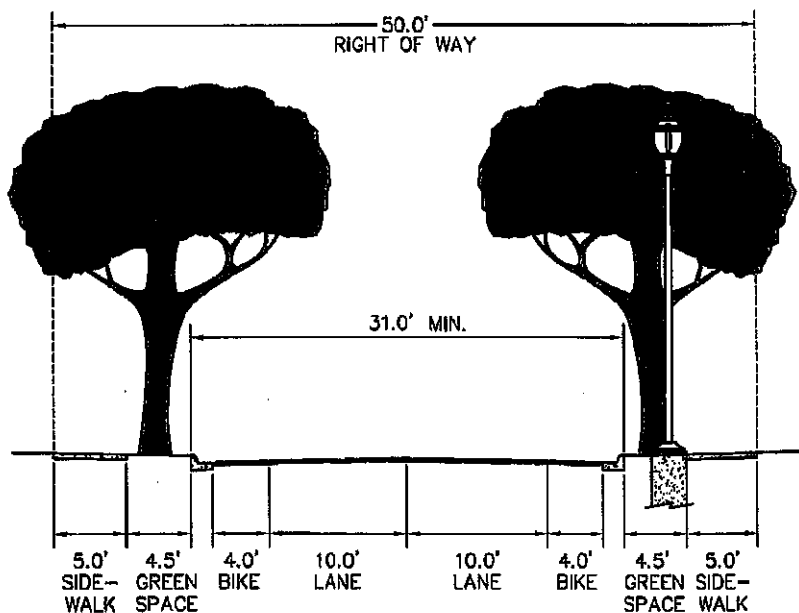
MINOR STREET



*ROAD CENTERLINE DOES NOT MATCH CENTERLINE OF RIGHT OF WAY.

- OR -

1. STANDARD HEIGHT TO THE STREET LIGHT HEAD SHALL BE 15 FEET (TYP).
2. STREET LIGHTS SHALL BE PELCO POST TOP LUMINAIRE POLES WITH BRISTOL SERIES, DARK SKY LED LUMINAIRES, OR STAFF APPROVED EQUAL, SPACED A MAXIMUM OF 200 FEET AND AT INTERSECTIONS. POLES AND LUMINAIRES SHALL BE POWDER COATED BRONZE FEDERAL STANDARD COLOR 20062. DIRECT BURIAL POLES NOT ALLOWED.
3. PAVING SECTION SHALL BE A MINIMUM OF 3-INCH HOT MIX ASPHALT AND 6-INCHES OF CLASS 7 AGGREGATE BASE (TO 1' BEYOND CURB) ON A STABLE SUBGRADE.
4. INCORPORATE BULBOUTS AND CURB EXTENSIONS AT INTERSECTIONS.
3. SEE SHEET 6 OF THE TYPICAL STREET SECTIONS FOR TREE SPACING. DO NOT PLACE TREES WITHIN 20' OF STREET LIGHTS. TREES SHALL BE CENTERED IN THE GREENSPACE.
4. ALL CURB IS 1.5' WIDE.
5. RAMP DRIVEWAYS UP TO SIDEWALK.



25 MPH
50' ROW



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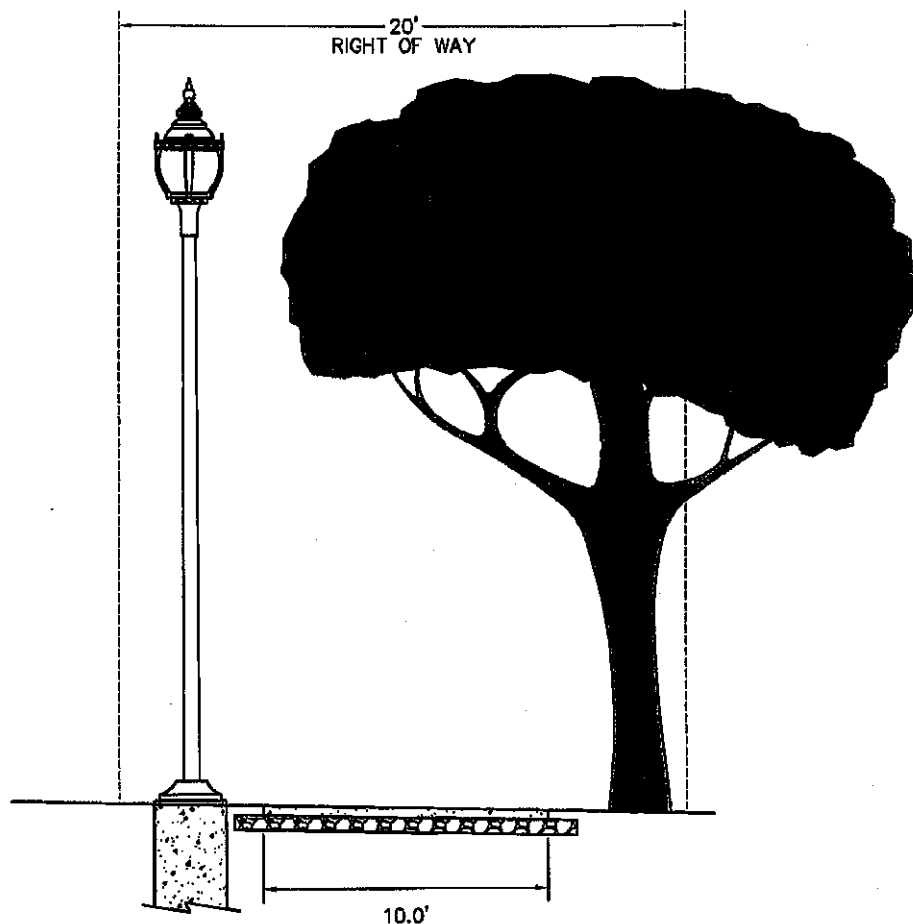
TYPICAL STREET SECTIONS

4 OF 6

NOT TO SCALE

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MULTIUSE TRAIL



1. STANDARD HEIGHT TO THE STREET LIGHT HEAD SHALL BE 14 FEET (TYP).
2. STREET LIGHTS SHALL BE PELCO POST TOP LUMINAIRE POLES WITH BRISTOL SERIES, DARK SKY LED LUMINAIRES, OR STAFF APPROVED EQUAL, SPACED A MAXIMUM OF 200 FEET AND AT INTERSECTIONS, POLES AND LUMINAIRES SHALL BE POWDER COATED BRONZE FEDERAL STANDARD COLOR 20062. DIRECT BURIAL POLES NOT ALLOWED.
3. SEE SHEET 6 OF THE TYPICAL STREET SECTIONS FOR TREE SPACING.
4. MULTI-USE TRAILS SHALL MATCH SIDEWALK DETAILS OTHER THAN AS CALLED OUT HEREIN.
5. CONTRACTION JOINTS SHALL BE PLACED EVERY 10'. THESE JOINTS SHALL BE SAW-CUT, TOOLED JOINTS WILL NOT BE ALLOWED.
6. EXPANSION JOINTS SHALL BE PLACED EVERY 100'.
7. CENTERSTRIPE SHALL BE SINGLE YELLOW SOLID OR DASHED PAINT (NOT THERMOPLASTIC).
8. CONCRETE SHALL BE THICKENED TO 6" IN ALL DRIVEWAYS.

20 MPH
20' ROW



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TYPICAL STREET SECTIONS

5 OF 6

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APPROVED STREET TREES

50' tall ≥4' from curb or sidewalk	30-50' tall ≥3' from curb or sidewalk	<20' tall ≥2' from curb or sidewalk
<ul style="list-style-type: none"> * Fruitless Kentucky Coffee Tree Ginkgo (male only) * Hackberry Japanese Zelkova (Green Vase) Lacebark Elm (Allee) Littleleaf Linden (Greenspire) London planetree Oak <ul style="list-style-type: none"> * Black * Northern Red Shingle * Shumard * Southern Red * Swamp White White * Willow * River Birch * Shortleaf Pine Silver Linden * Sugarberry (Sugar Hackberry) Turkish Filbert 	<ul style="list-style-type: none"> * American Hophornbeam American Holly Chinese Pistache English Oak Hawthorns Kousa Dogwood Persian Ironwood * Scarlet Oak Thornless Honeylocust (Shademaster) * Yellowwood 	<ul style="list-style-type: none"> * American Smoketree Amur Maple Crabapple Hedge Maple Magnolia (Little Gem) Nellie Stevens holly Possumhaw * Redbud Shadlow serviceberry Yaupon Holly

* - DENOTES A NATIVE SPECIES
() - DENOTES A RECOMMENDED VARIETY

1. SHALLOW ROOTED TREES SUCH AS MAPLE, ASH, SWEETGUM, TULIP TREE, PIN OAK AND POPLAR SHALL NOT BE PLANTED WITHIN 10' OF A CURB OR SIDEWALK.
2. USE OF ANY TREE NOT SHOWN ON THIS LIST REQUIRES APPROVAL BY CITY STAFF.
3. CRITERIA FOR TREES SHALL MATCH THOSE SHOWN IN THE LARGE SCALE DEVELOPMENT ORDINANCE SECTION 14-256.
4. TREE SPACING SHALL BE 30' FOR TREES LESS THAN 20' TALL, 40' FOR TREES 30-50' TALL AND 50' FOR TREES 50' OR TALLER OR AS REQUIRED BY ORDINANCE 18-259. TREE HEIGHTS ARE MATURE TREE HEIGHTS.
5. ONLY TREES ON THE <20' TALL LIST MAY BE USED NEAR OR UNDER OVERHEAD UTILITIES.
6. TREES SHALL NOT BE PLANTED WITHIN 20' OF LIGHT POLE.
7. TREES SHALL BE CENTERED IN THE GREENSPACE.



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TYPICAL STREET SECTIONS

6 OF 6

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